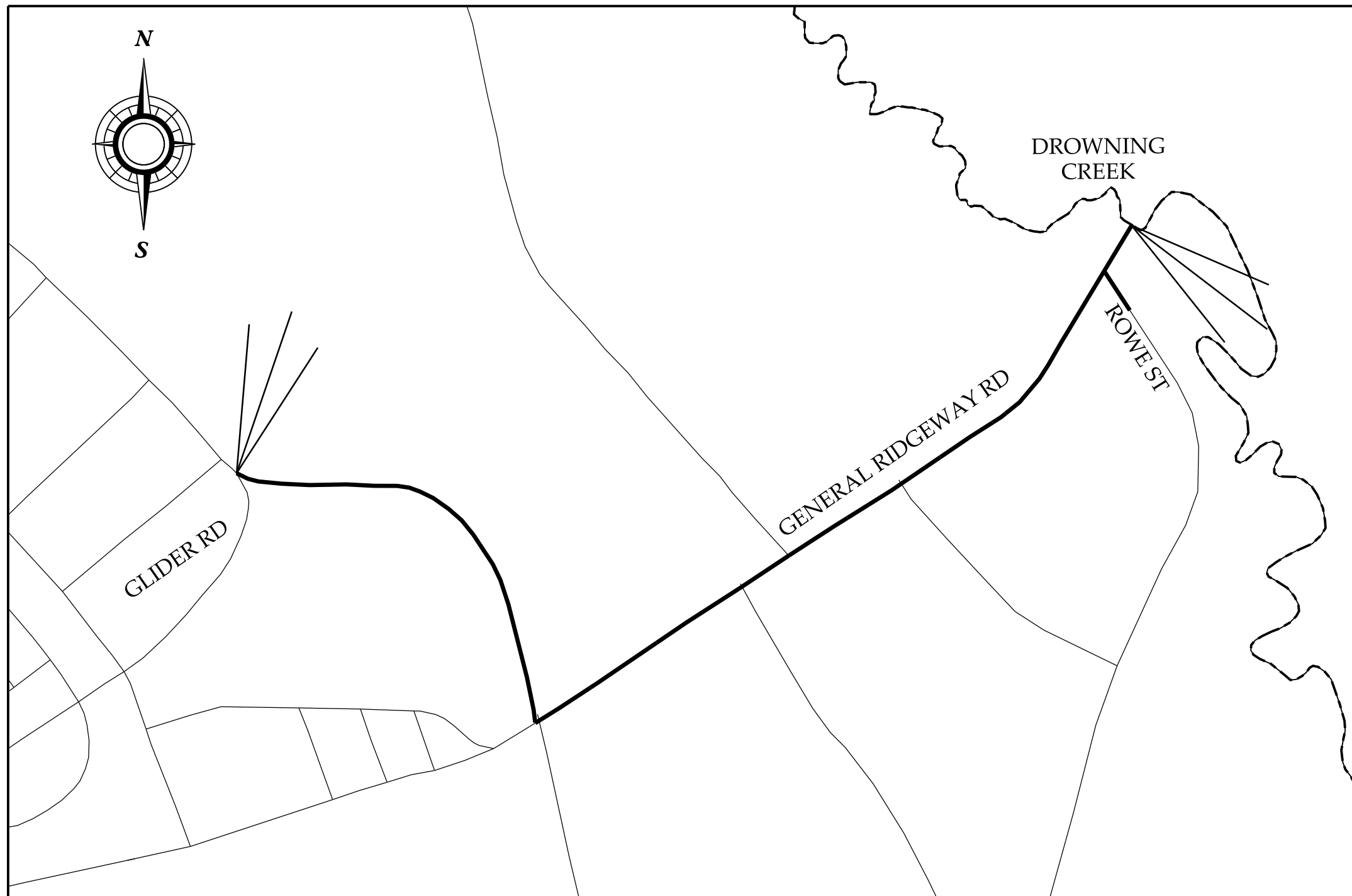


# MAP 1 - GENERAL RIDGEWAY RD

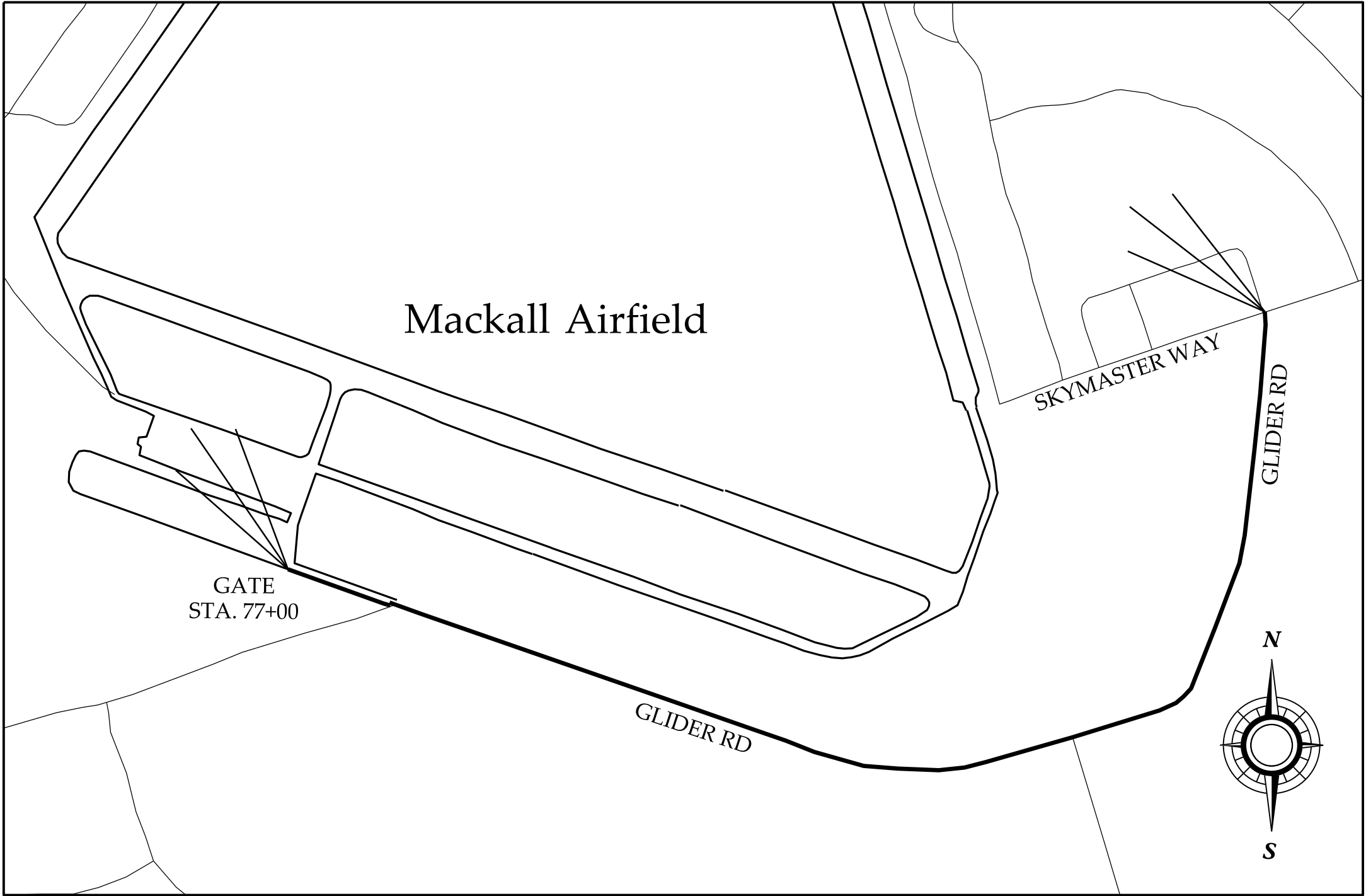


# MAP 2 - GLIDER RD



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23-Rev01 05/08/2022  
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# MAP 3 - GLIDER RD



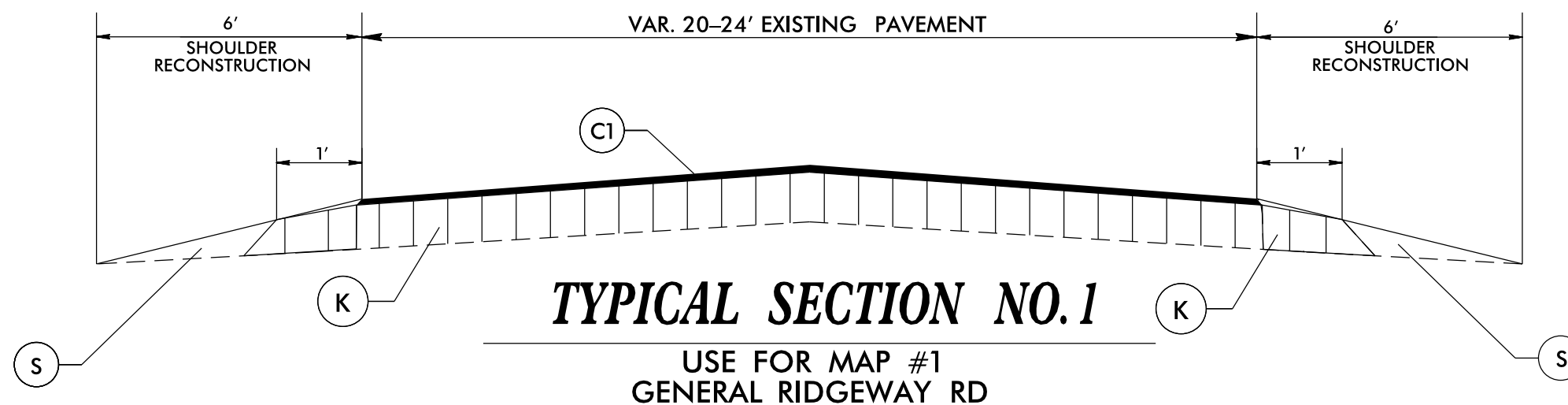
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-23-Result  
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mpeckall

# PROJECT NOTES

1. RECLAMATION WIDTH SHALL BE A MINIMUM OF TWO FEET WIDER THAN PROPOSED PAVEMENT (1' ON EACH SIDE OF ROADWAY), INCLUDING ALL WIDENING AREAS.
2. VEGETATION MUST BE REMOVED FROM SHOULDER 1 FOOT BEYOND PROPOSED EDGE OF PAVEMENT PRIOR TO FDR.
3. A TACK COAT BETWEEN THE FDR LAYER AND THE SURFACE COURSE LAYER IS REQUIRED.
4. PAVE A MINIMUM 3' APRON AT ALL UNPAVED SIDE ROADS.
5. MAINTAIN ONE LANE OPEN AT ALL TIMES DURING CONSTRUCTION. LOCAL LIGHTWEIGHT TRAFFIC TO BE ALLOWED ON CEMENT TREATED BASE PRIOR TO PAVING.
6. CONTRACTOR SHALL WET CURE FDR FOR 7 DAYS PRIOR TO PAVING. DO NOT LET THE SURFACE OF FDR DRY OUT.
7. MAP 2 – PLACE HIGH VISIBILITY CROSSWALK AT INTERSECTION OF GLIDER RD AND STATION HOSPITAL RD.
8. MAP 3 – APPROXIMATE LOCATIONS FOR PROPOSED DITCHES AND PIPES SHOWN ON PLANS FOR REFERENCE ONLY. ACTUAL LOCATIONS TO BE DETERMINED IN FIELD BY ENGINEER.

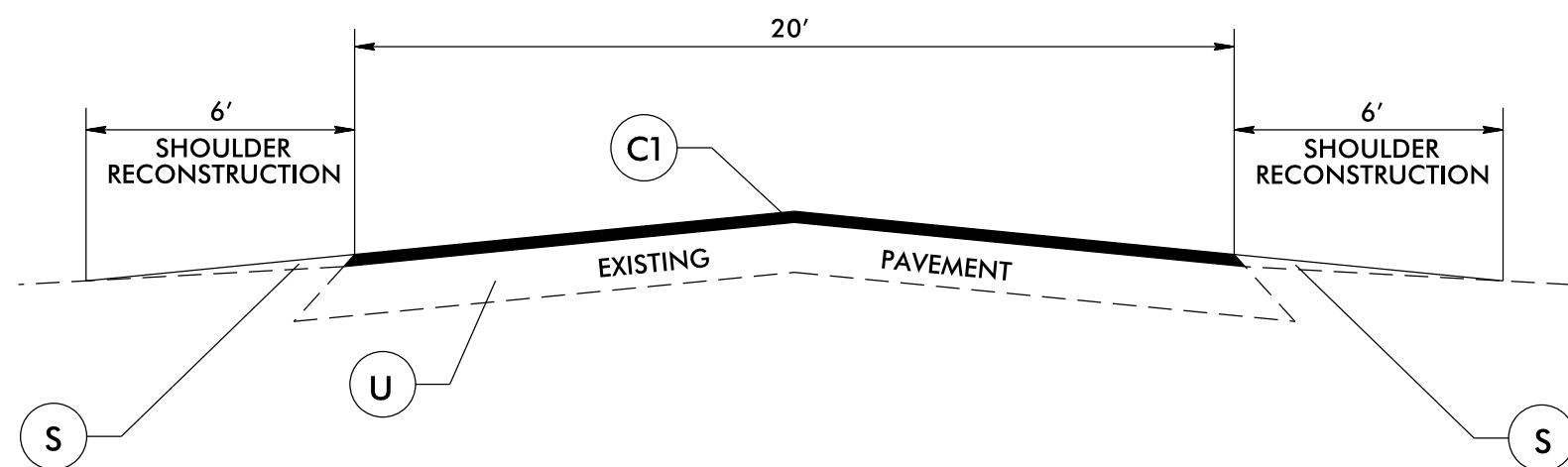
## PAVEMENT SCHEDULE

C1	PROP. APPROX. 3.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD. IN EACH OF TWO LAYERS
J	FOR ALL WIDENING AREAS REMOVE MATERIAL TO 8" DEPTH AND REPLACE WITH AGGREGATE BASE COURSE
K	PROP. 12" FULL DEPTH RECLAMATION USING A CEMENT RATE OF 55 LBS. PER SQ. YD.
S	AGGREGATE SHOULDER BORROW (ASB)
U	EXISTING PAVEMENT
V1	1.5" MILLING



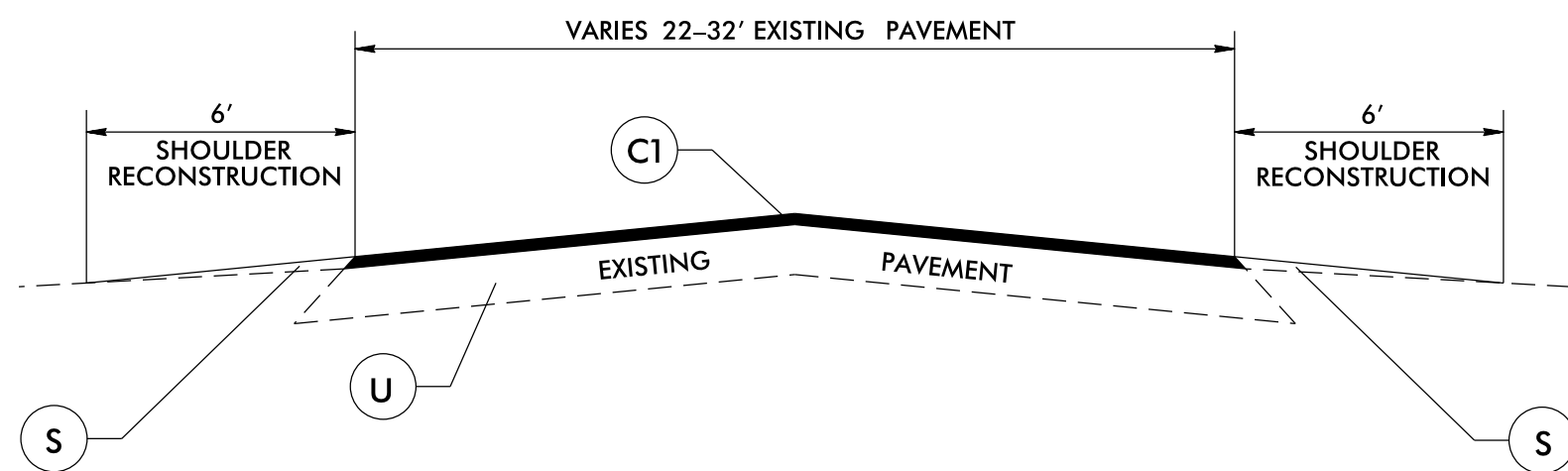
### PAVEMENT SCHEDULE

<b>C1</b>	PROP. APPROX. 3.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD. IN EACH OF TWO LAYERS
<b>J</b>	FOR ALL WIDENING AREAS REMOVE MATERIAL TO 8" DEPTH AND REPLACE WITH AGGREGATE BASE COURSE
<b>K</b>	PROP. 12" FULL DEPTH RECLAMATION USING A CEMENT RATE OF 55 LBS. PER SQ. YD.
<b>S</b>	AGGREGATE SHOULDER BORROW (ASB)
<b>U</b>	EXISTING PAVEMENT
<b>VI</b>	1.5" MILLING



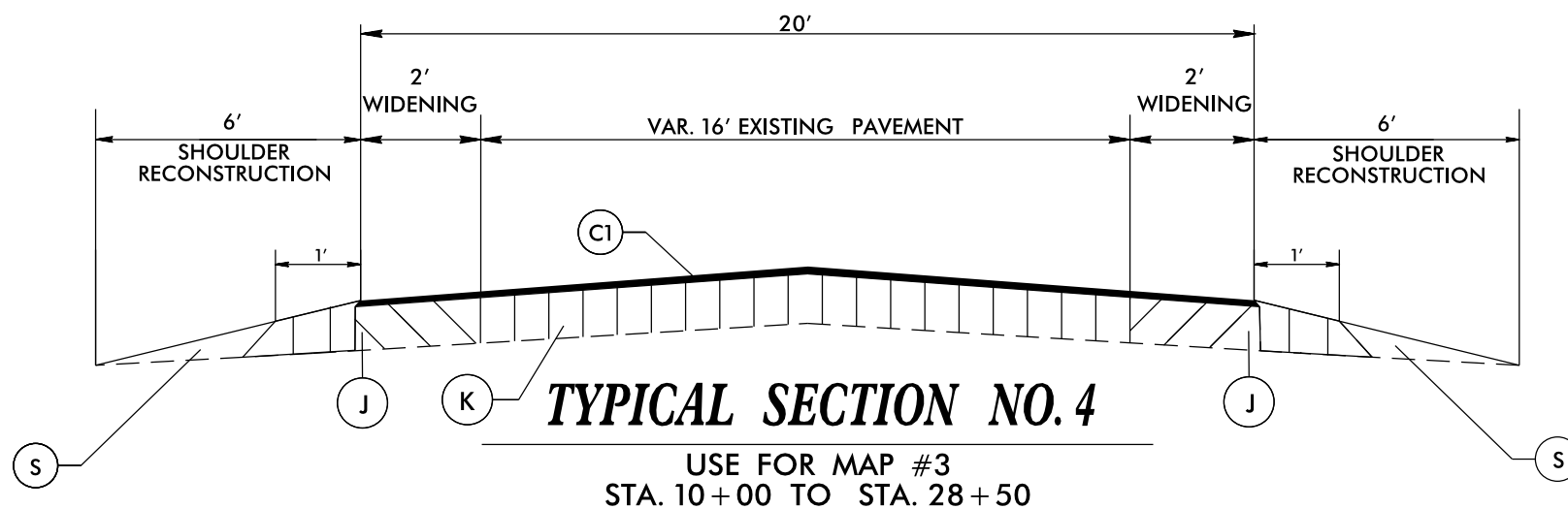
### TYPICAL SECTION NO. 2

USE FOR MAP #1  
SECTION OF ROWE ST FOR 120' FROM  
GENERAL RIDGEWAY RD TO BEGINNING  
OF CONCRETE PAVEMENT

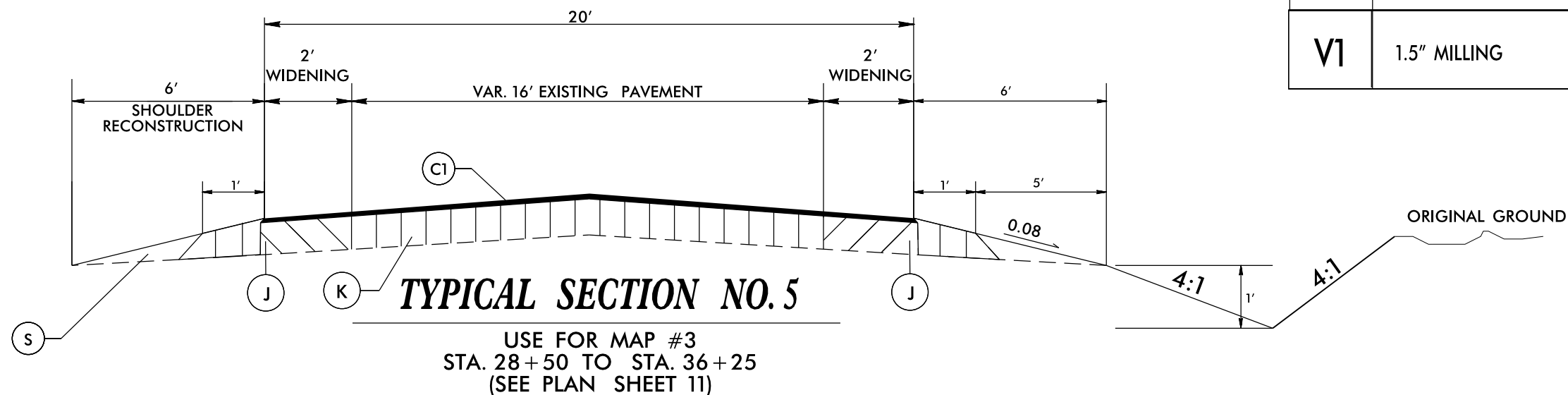


### TYPICAL SECTION NO. 3

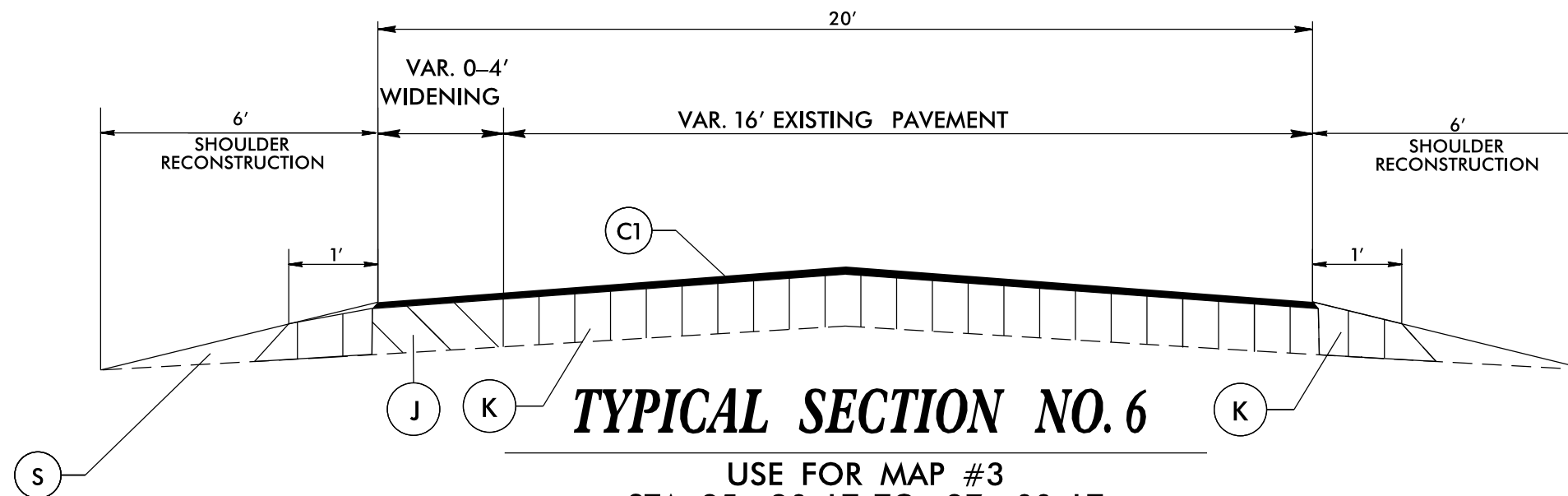
USE FOR MAP #2



PAVEMENT SCHEDULE	
C1	PROP. APPROX. 3.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD. IN EACH OF TWO LAYERS
J	FOR ALL WIDENING AREAS REMOVE MATERIAL TO 8" DEPTH AND REPLACE WITH AGGREGATE BASE COURSE
K	PROP. 12" FULL DEPTH RECLAMATION USING A CEMENT RATE OF 55 LBS. PER SQ. YD.
S	AGGREGATE SHOULDER BORROW (ASB)
U	EXISTING PAVEMENT
VI	1.5" MILLING



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**TYPICAL SECTION NO. 6**

USE FOR MAP #3  
 STA. 35+20 LT TO 37+00 LT  
 TAPER WIDENING FROM 0-4'  
  
 STA. 37+00 LT TO GATE AT STA. 77+00 LT  
 FULL 4' WIDENING  
 MAINTAIN 9' OFFSET FROM  
 PROPOSED RIGHT EOP TO EXISTING FENCE.

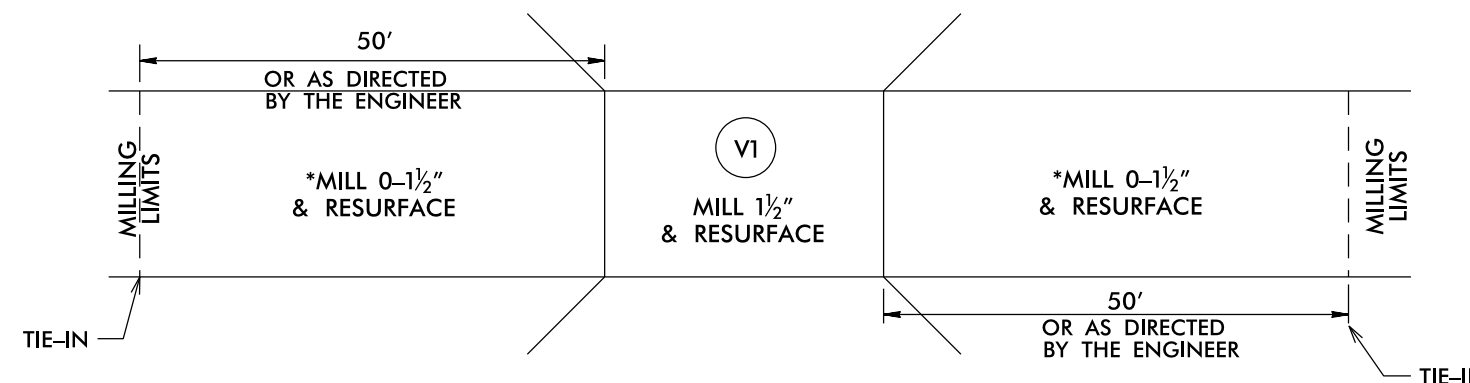
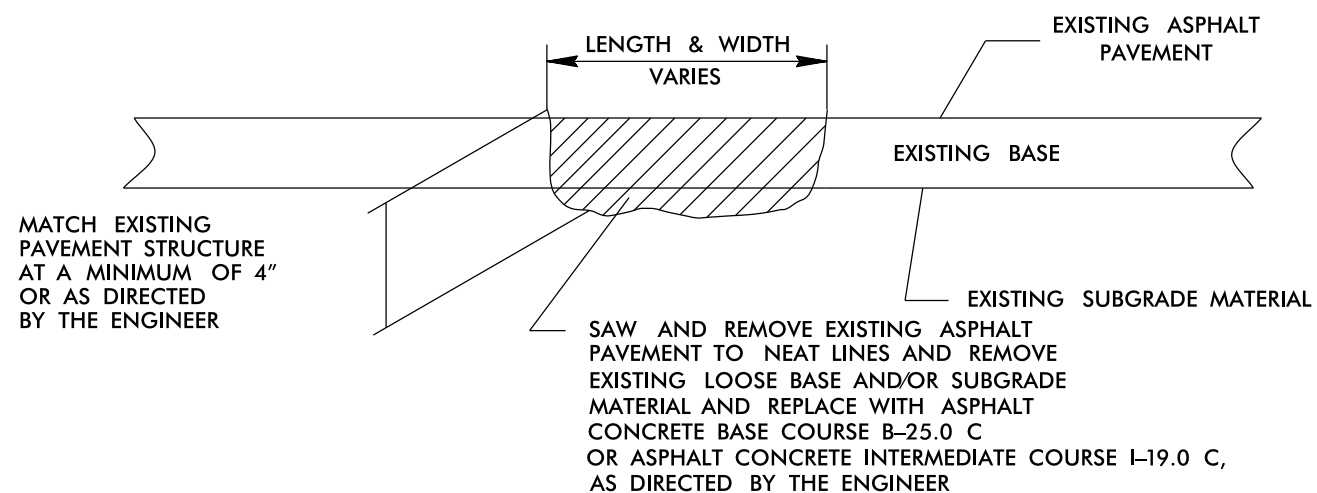
**PAVEMENT SCHEDULE**

<b>C1</b>	PROP. APPROX. 3.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD. IN EACH OF TWO LAYERS
<b>J</b>	FOR ALL WIDENING AREAS REMOVE MATERIAL TO 8" DEPTH AND REPLACE WITH AGGREGATE BASE COURSE
<b>K</b>	PROP. 12" FULL DEPTH RECLAMATION USING A CEMENT RATE OF 55 LBS. PER SQ. YD.
<b>S</b>	AGGREGATE SHOULDER BORROW (ASB)
<b>U</b>	EXISTING PAVEMENT
<b>V1</b>	1.5" MILLING



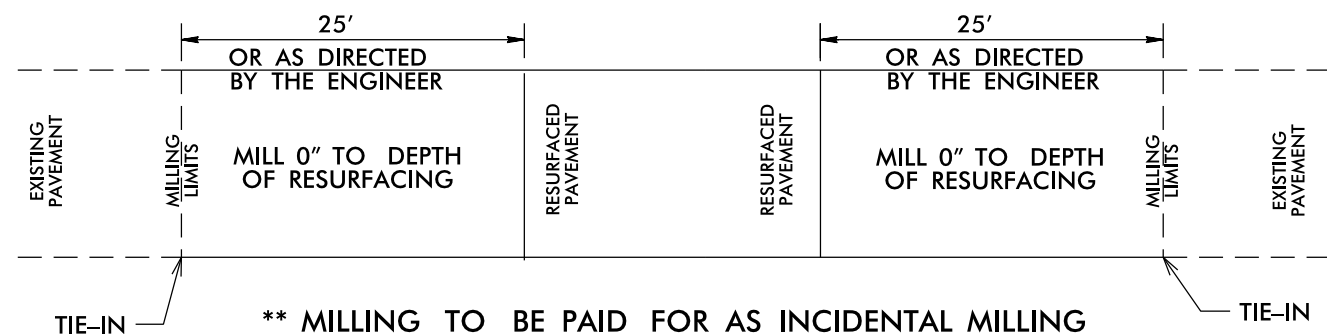
# DETAILS

## DETAILS OF PATCHING EXISTING PAVEMENT PRIOR TO RESURFACING



## BRIDGE DRAWING FOR MAP NO 2 GLIDER RD

\* MILLING FOR APPROACHES SHALL BE PAID FOR UNDER INCIDENTAL MILLING



\*\* MILLING TO BE PAID FOR AS INCIDENTAL MILLING

## PAVEMENT TIE-IN DETAIL

REVISIONS

8/17/99

\$\$\$\$\$SYTIME\$\$\$\$\$  
 \$\$\$EDGN\$\$\$\$\$  
 \$\$\$\$\$\$\$\$

MATCH LINE 10 + 00 Ext. -50.00

POT Sta. 10+00.00

10

GLIDER ROAD

15

20

S 5.20 34.4 W

MATCH LINE 23 + 50

PROJECT REFERENCE NO.	SHEET NO.
49218.2	10
RW SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER




These plans were created using GIS and /or NC ONE Map data. No survey was used in the creation of these plan sheets. Alignment is for reference only

PROJECT REFERENCE NO.	SHEET NO.
492182	11
RW SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

REVISIONS




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8/17/99

##### SYSTEMS #####  
##### PLANNING #####  
##### DESIGN #####  
##### CONSTRUCTION #####

PROJECT REFERENCE NO.	SHEET NO.
492182	12
RW SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

8/1/2019

REVISIONS

MATCH LINE 37+50

MATCH LINE 51+50

# 45 GLIDER ROAD

40

50

S 72° 57' 16" W

S 88° 22' 27" W



These plans were created using GIS and /or NC ONE Map data. No survey was used in the creation of these plan sheets. Alignment is for reference only

\$\$\$\$\$SYTIME\$\$\$\$\$  
\$\$\$\$\$DATE\$\$\$\$\$  
\$\$\$\$\$DRAWN\$\$\$\$\$  
\$\$\$\$\$CHECKED\$\$\$\$\$  
\$\$\$\$\$DATE\$\$\$\$\$  
\$\$\$\$\$BY\$\$\$\$\$

8/17/99

REVISIONS


\$\$\$\$\$SYTIME\$\$\$\$\$  
\$\$\$\$\$DATE\$\$\$\$\$  
\$\$\$\$\$DRAWN\$\$\$\$\$  
\$\$\$\$\$CHECKED\$\$\$\$\$  
\$\$\$\$\$APPVED\$\$\$\$\$

MATCH LINE 51+50



MATCH LINE 65+50

PROJECT REFERENCE NO.	SHEET NO.
49218.2	13
RW SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

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8/17/99

REVISIONS

PROJECT REFERENCE NO.	SHEET NO.
49218.2	14
RW SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

MATCH LINE 65+50

MATCH LINE 79+50

# GLIDER ROAD

N 70° 27' 16" W

70

75

END MAP 3  
AT GATE  
STA. 77+00

PLACE BORROW MATERIAL UNIFORMLY  
AND AS DIRECTED BY THE ENGINEER  
STA. 73+50 TO STA. 76+50  
APPROXIMATELY 350 CY

BEGIN PROPOSED DITCH  
-L- RT STA. 74+25

END PROPOSED DITCH  
-L- RT STA. 75+75

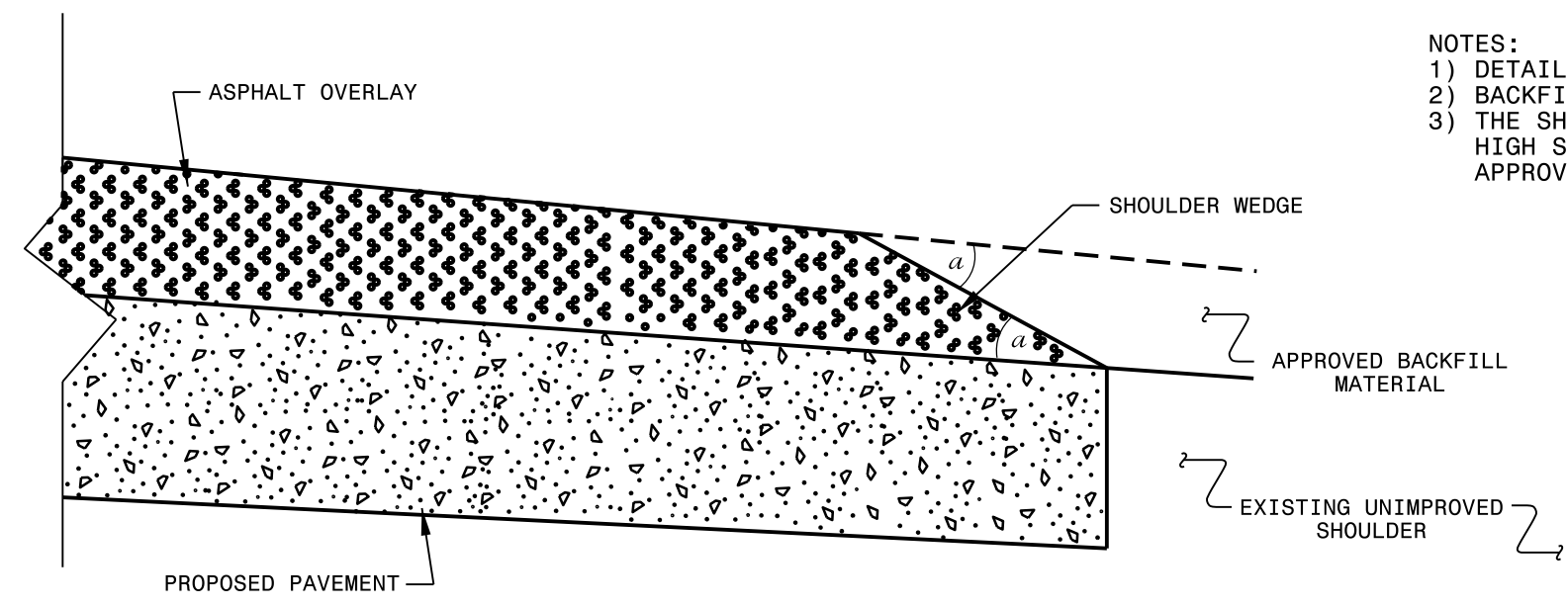
2 - 24" RCP



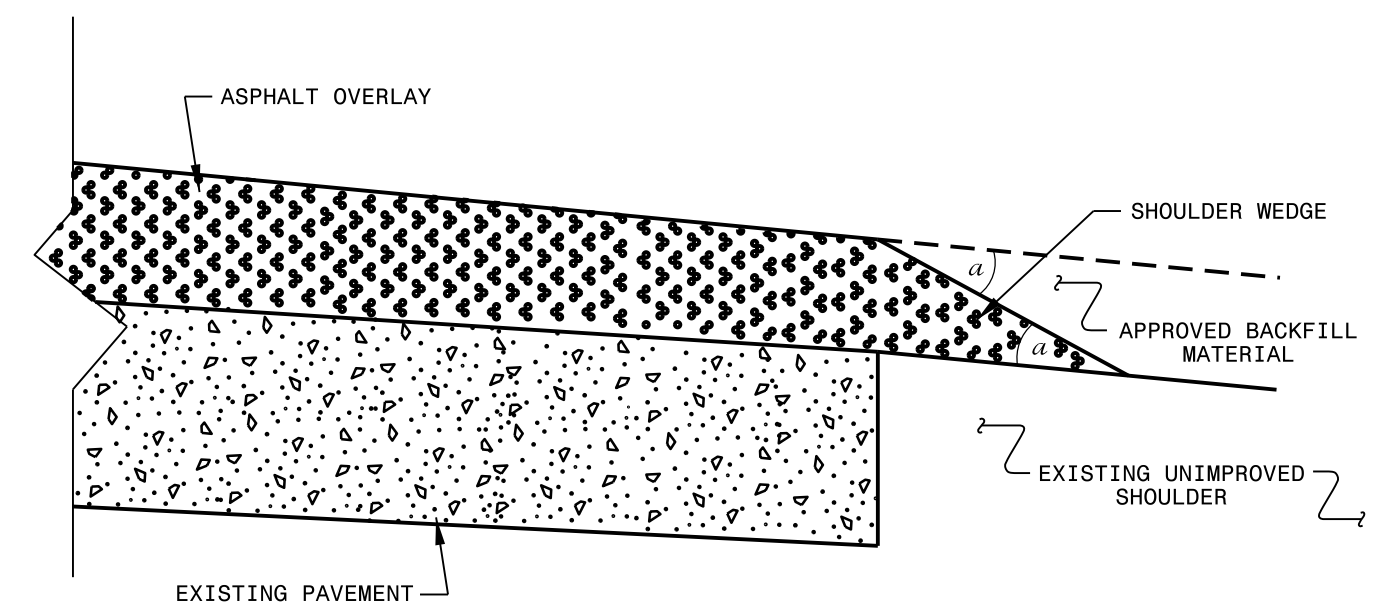
These plans were created using GIS  
and /or NC ONE Map data.  
No survey was used in the creation  
of these plan sheets.  
Alignment is for reference only

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\$\$\$\$\$DRAWN\$\$\$\$\$  
\$\$\$\$\$CHECKED\$\$\$\$\$

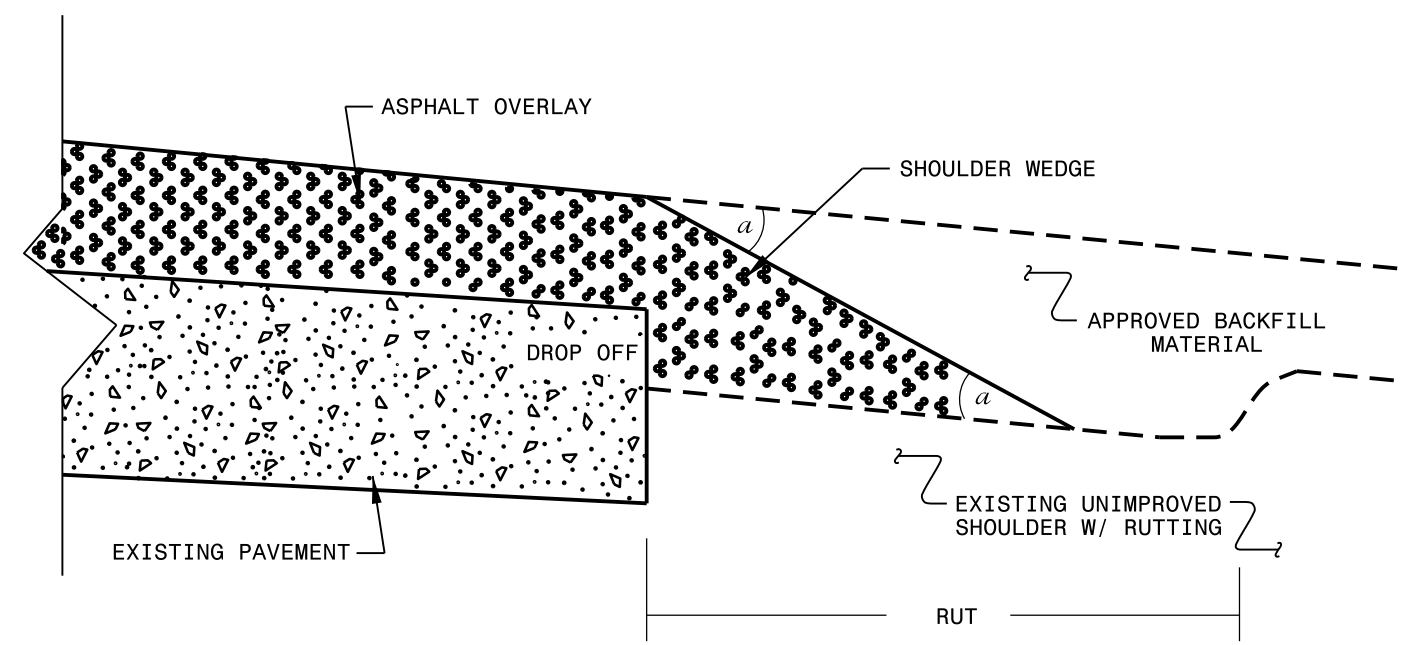
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
  - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
  - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ Widening or  
 with Existing Paved Shoulder having no dropoffs)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ NO Widening)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Adjacent to  
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>	
Office 919-707-6950 FAX 919-250-4119	
<b>SHOULDER WEDGE DETAILS</b>	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 2/2/16
CHECKED BY:	DATE:
FILE SPEC.: s:\usr\detatl1s\stand\shoulderwedgedetatl1.dgn	

DOCUMENT NOT CONSIDERED FINAL  
UNLESS ALL SIGNATURES COMPLETED

\$\$\$\$SYTIME\$\$\$\$  
 \$\$\$USERNAME\$\$\$

PROJECT NO.	SHEET NO.	TOTAL NO.
49218.2	16	

### SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGT H	WIDT H	0043000000-N GRADING	0106000000-E BORROW EXCAVATION	0241000000-E GENERIC GRADING ITEM, SOIL CEMENT BASE (FULL DEPTH RECLAMATION)	0318000000-E FOUNDATION CONDITIONING MATERIAL, MINOR STRUCTURES	0320000000-E FOUNDATION CONDITIONING FABRIC	0448300000-E 18" RC PIPE CULVERT, CLASS IV	0448400000-E 24" RC PIPE CULVERTS, CLASS IV	1121000000-E AGGREGATE BASE COURSE	1187000000-E PORTLAND CEMENT FOR SOIL CEMENT BASE	1245000000-E SHOULDER RECONSTRUCTION	1260000000-E AGGREGATE SHOULDER BORROW	1297000000-E 1.5" MILLING	1330000000-E INCIDENTAL MILLING	1519000000-E SURFACE COURSE, S9.5B	1575000000-E ASPHALT BINDER FOR PLANT MIX	1704000000-E PATCHING EXISTING PAVEMENT	3649000000-E RIP RAP, CLASS B	3656000000-E GEOTEXTILE FOR DRAINAGE	6071010000-E WATTLE	6084000000-E SEED & MULCHING		
																																MI	FT
49218.2	Richmond	1	GENERAL RIDGEWAY RD.	FROM CJ AT BRIDGE TO GLIDER ROAD (INCLUDES 120' SECTION OF ROWE ST)	1,2	2	ZWU	NO	NO	1.06	22	*		16,043						441	2.12	650.00			2,965	199							
<b>TOTAL FOR MAP NO. 1</b>										<b>1.06</b>				<b>16,043</b>						<b>441</b>	<b>2.12</b>	<b>650.00</b>			<b>2,965</b>	<b>199</b>							
49218.2	Richmond	2	GLIDER ROAD	FROM 11 TH AIRBORN DIVISION RD TO SKYMASTER WAY	3	2	ZWU	NO	NO	0.84	22										1.71	479.00	160	1,475	2,335	163	142						
<b>TOTAL FOR MAP NO. 2</b>										<b>0.84</b>											<b>1.71</b>	<b>479.00</b>	<b>160</b>	<b>1,475</b>	<b>2,335</b>	<b>163</b>	<b>142</b>						
49218.2	Richmond	3	GLIDER ROAD	FROM SKYMASTER WAY TO GATE AT STA. 77+00	4,5,6	2	ZWU	NO	NO	1.27	20			350	16,380	18	70	56	72	1,350	450	2.54	711.00			2,460	165		20	30	100	1.00	
<b>TOTAL FOR MAP NO. 3</b>										<b>1.27</b>				<b>350</b>	<b>16,380</b>	<b>18</b>	<b>70</b>	<b>56</b>	<b>72</b>	<b>1,350</b>	<b>450</b>	<b>2.54</b>	<b>711.00</b>			<b>2,460</b>	<b>165</b>		<b>20</b>	<b>30</b>	<b>100</b>	<b>1.00</b>	
<b>TOTAL FOR PROJ NO. 49218.2</b>										<b>3.17</b>		<b>1.000</b>	<b>350</b>	<b>32,423</b>	<b>18</b>	<b>70</b>	<b>56</b>	<b>72</b>	<b>1,350</b>	<b>891</b>	<b>6.37</b>	<b>1,840.00</b>	<b>160</b>	<b>1,475</b>	<b>7,760</b>	<b>527</b>	<b>142</b>	<b>20</b>	<b>30</b>	<b>100</b>	<b>1.00</b>		
<b>GRAND TOTAL</b>										<b>3.17</b>		<b>1.000</b>	<b>350</b>	<b>32,423</b>	<b>18</b>	<b>70</b>	<b>56</b>	<b>72</b>	<b>1,350</b>	<b>891</b>	<b>6.37</b>	<b>1,840.00</b>	<b>160</b>	<b>1,475</b>	<b>7,760</b>	<b>527</b>	<b>142</b>	<b>20</b>	<b>30</b>	<b>100</b>	<b>1.00</b>		



PROJECT NO.	SHEET NO.	TOTAL NO.
49218.2	17	

**THERMOPLASTIC AND PAINT QUANTITIES**

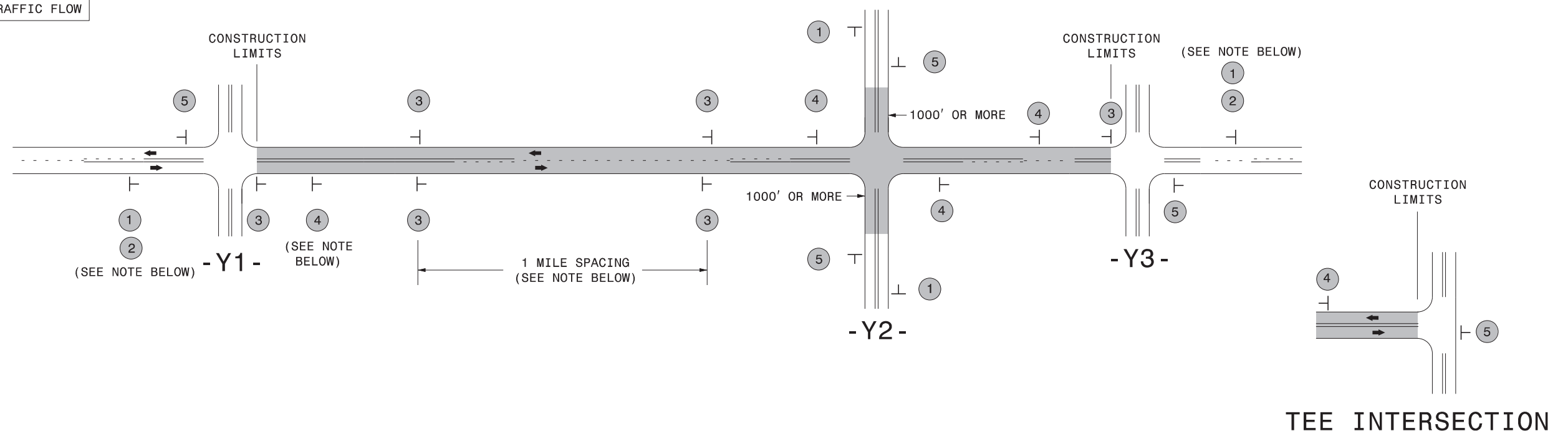
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	4413000000-E	4457000000-N	4685000000-E		4891000000-E	4905000000-N		
								LENGTH	WIDTH	WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	4" X 90 M WHITE THERMO	4" X 90 M YELLOW THERMO	GENERIC PAVEMENT MARKING ITEM - 24" X 90 M WHITE THERMO	SNOW PLOWABLE MARKERS Y & Y MARKERS
								MI	FT	SF	LS	LF	LF	LF	EA
49218.2	Richmond	1	GENERAL RIDGEWAY RD.	FROM CJ AT BRIDGE TO GLIDER ROAD (INCLUDES 120' SECTION OF ROWE ST)	1,2	2	2WU	1.06	22	119					77
<b>TOTAL FOR MAP NO. 1</b>								<b>1.06</b>		<b>119</b>		<b>12,937</b>	<b>11,960</b>		<b>77</b>
49218.2	Richmond	2	GLIDER ROAD	FROM 11 TH AIRBORN DIVISION RD TO SKYMASTER WAY	3	2	2WU	0.84	22	94	*				62
<b>TOTAL FOR MAP NO. 2</b>								<b>0.84</b>		<b>94</b>		<b>9,045</b>	<b>9,100</b>	<b>85</b>	<b>62</b>
49218.2	Richmond	3	GLIDER ROAD	FROM SKYMASTER WAY TO GATE AT STA. 77+00	4,5,6	2	2WU	1.27	20	170					84
<b>TOTAL FOR MAP NO. 3</b>								<b>1.27</b>		<b>170</b>		<b>10,760</b>	<b>10,760</b>		<b>84</b>
<b>TOTAL FOR PROJ NO. 49218.2</b>								<b>3.17</b>		<b>383</b>	<b>1</b>	<b>32,742</b>	<b>31,820</b>	<b>85</b>	<b>223</b>
												<b>64,562</b>			
<b>GRAND TOTAL</b>								<b>3.17</b>		<b>383</b>	<b>1</b>	<b>32,742</b>	<b>31,820</b>	<b>85</b>	<b>223</b>
												<b>64,562</b>			

# SIGNING FOR RESURFACING PROJECTS

**LEGEND**

┃ STATIONARY SIGN

← DIRECTION OF TRAFFIC FLOW



## MAINLINE (-L-) SIGNING

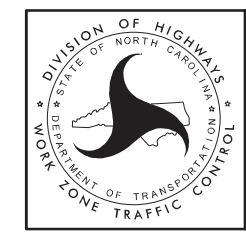
## -Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">               PLACED 500' IN ADVANCE OF FLAGGER.         </div> <div style="text-align: center;">               PLACED 250' IN ADVANCE OF FLAGGER.         </div> </div>
	2		#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3		- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	4		- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	5		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

### MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.



**ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING**

5/15/2017 S:\TUXWZTC\Resurfacing\2L2W & AST Resurfacing Details\Resurfacing\_AdvWarn\_2Ln.dgn User:kedais